

January 30, 2025

Attention: David Horwood

Representative Holdings Inc.
c/o David Horwood
242 Main Street East,
Hamilton, ON L8N 1H5

**Re: Response Letter – Transportation Study Addendum
117 Forest Avenue and 175 Catharine Street South
City of Hamilton
Our Project No. NT-16-103**

NexTrans Consulting Engineers (A Division of NextEng Consulting Group Inc.) was retained by Representative Holdings Inc. (The Client) to prepare this Response Letter to address the City's comments for the proposed Zoning By-law Amendment application and site plan application. The subject sites are located at municipal addresses 117 Forest Avenue and 175 Catharine Street South, in the City of Hamilton.

NexTrans has prepared a Transportation Impact Study dated October 2023 and an Addendum dated July 25, 2024 in support of the proposed development application mentioned above. The City of Hamilton has provided comments dated November 20, 2024 (**Appendix A**).

1.0 Comments and Responses

As indicated, the following comments have been received from the City of Hamilton and appropriate responses are provided, along with technical analysis provided in this Response Letter:

1. Transportation Planning notes a Transportation Assessment (TA) was previously submitted and approved as of April 22, 2024. It was noted that any revisions to the site plan to include additional residential units shall require an updated TA in order to reflect the proposed modifications to the site plan moving forward.

Response: Noted. No further action is required.

2. A revised Transportation letter was provided as part of the formal submission package. Transportation Planning notes that the letter appears to be 'incomplete' as references to figures (Figures 5A & 5B) and comments indicating certain revisions to the site plan (Comment #9 indicating revisions and additional details provided at the driveway access to Young Street) have not been completed.

Response: Noted. It has been addressed in **Comment #9** below in this Response Letter.

3. Considering the increase in density being proposed within the subject lands, Transportation Planning shall require a provision of funds to the amount of \$10,000 for the purpose of installing future traffic calming measures within the surrounding area of the proposed development, subject to the satisfaction and approval of the Manager, Transportation Planning as part of the future conditions of Site Plan approval.

At a time when the Applicant is prepared to provide the required funds, Transportation Planning will correspond with the Transportation Operations & Maintenance department to provide an account number for the funds to be deposited accordingly.

Response: Noted. This fund will be provided as part of the site plan agreement.

- Transportation Planning requires short-term and long-term bicycle parking to be provided as per the requirements of the City of Hamilton Comprehensive Zoning By-law 05-200, Section 5: Parking. All short-term and long-term bicycle parking shall be clearly indicated, illustrated and the total number of spaces shall be numbered on the site plan accordingly.

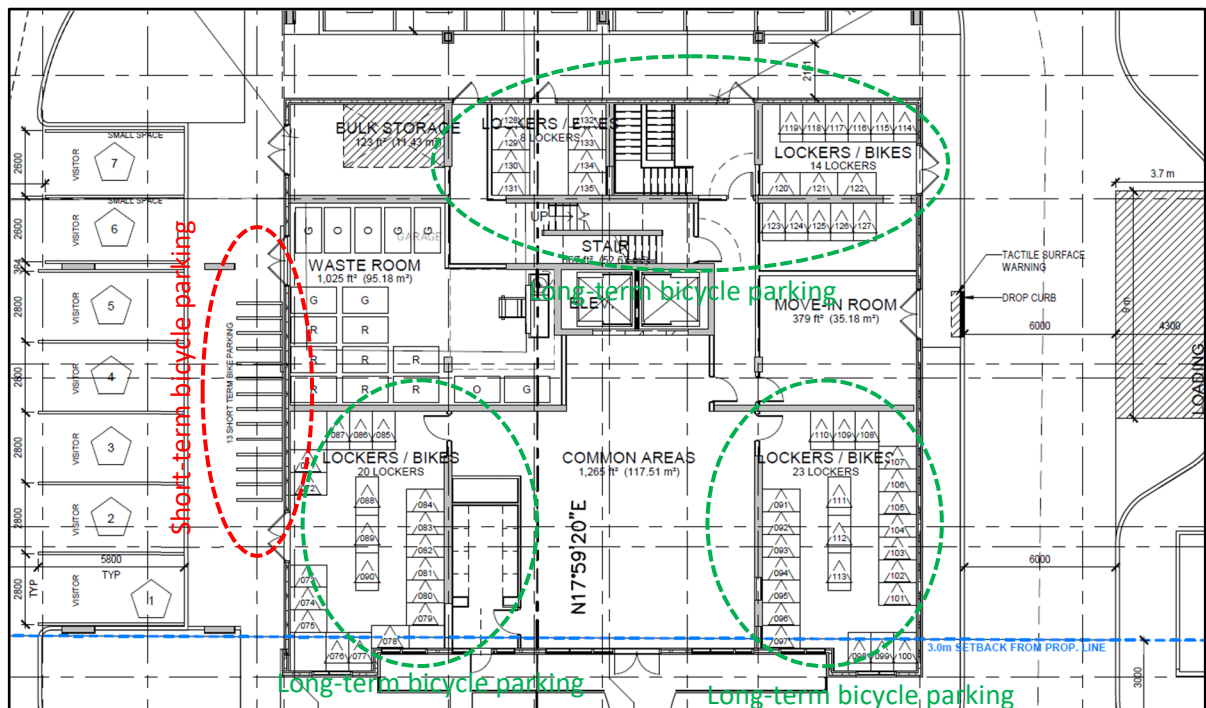
Response: Noted. This comment has been addressed in Table 1 below. All short-term and long-term bicycle parking spaces are indicated on the site plan, as illustrated in Figure 1 and Appendix B.

Table 1 – Zoning By-law No. 05-200 Bicycle Parking Requirement

| Land Use | No. of Unit/GFA | Long-Term Spaces | Short-Term Spaces | Total |
|-------------|-----------------|--|--|------------|
| Residential | 256 units | 0.5 spaces/dwelling unit 128 spaces | 0.05 spaces/dwelling unit 13 spaces | 131 spaces |

Based on the current Zoning By-law requirement, the proposed development requires 129 bicycle parking spaces (5 short-term and 124 long-term). The proposed development will provide a total of 124 long-term and 5 short-term spaces, for a total of 129 bicycle parking spaces. This meets the Zoning By-law requirement and this provision will encourage future residents to use active mode of transportation to and from the proposed development.

Figure 1 – Short-Term and Long-Term Bicycle Parking Spaces



5. Transportation Planning recommends additional Transportation Demand Management (TDM) measures to be provided:

- Provision of preloaded Presto/Transit passes with the purchase of an individual residential unit;

Response: As indicated in the October 2023 Transportation Impact Study and August 2024 Addendum, the proposed development has excellent access to the public transit and only located approximately 400 m (or less than 6-minute walk) to the Hamilton GO Train Station and transit terminal. The proposed development is also located within a few minutes walk to bus stops for HSR Bus Routes 22, 23, 24, 25, 26, 27. Therefore, the future residents who choose to live in this building will be most like take transit instead of driving given the easy access to public transit.

Therefore, on this basis, this City's recommendation is not required. This comment has been addressed.

- Reducing the number of passenger vehicle parking stalls provided to the minimum required through the City of Hamilton Comprehensive Zoning By-Law 05-200, Section 5: Parking;

Response: The proposed development only provides a total of 136 vehicle parking spaces for proposed 256 residential units. This is only approximately 0.53 spaces/unit. Therefore, this comment has been addressed.

- Unbundled the cost of an individual parking stall from the cost of a residential unit; and

Response: Noted and the proposed development will Unbundled the cost of an individual parking stall from the cost of a residential unit

- Provision of Electric Vehicle (EV) charging stalls within the underground parking area.

Response: This comment will be addressed as part of the site plan stage.

6. The existing right-of-way at the subject property along Catharine Street South and Forest Avenue is approximately ± 20.0 metres. Transportation Planning does not require right-of-way dedications to be provided as the existing right-of-way width meets the requirements of the Council Approved Urban Official Plan: Chapter C - City Wide Systems and Designations, 4.5 Road Network Functional Classification, 4.5.2. Local Roads (Catharine Street South and Forest Avenue).

Response: Noted. No further action is required at this time.

7. Catharine Street South & Forest Avenue are both Local Roads. The Applicant is to dedicate a 4.57 metres x 4.57 metres Daylighting Triangle to the right-of-way, as per the Council Approved Urban Official Plan: Chapter C - City Wide Systems and Designations 4.5 Road Network Functional Classification; Daylighting Triangles 4.5.7.

Response: This comment has been addressed in the revised site plan (Appendix B), as per **Figure 2**.

8. Catharine Street South & Young Street are both Local Roads. The Applicant is to dedicate a 4.57 metres x 4.57 metres Daylighting Triangle to the right-of-way, as per the Council Approved Urban Official Plan: Chapter C - City Wide Systems and Designations 4.5 Road Network Functional Classification; Daylighting Triangles 4.5.7.

Response: It should be noted that this requirement is not possible as both the existing transformer and building bearing wall will remain and will not be altered as part of the site redevelopment. Please see **Figure 3** for more information. Therefore, the required daylight triangle cannot be provided and it will be operating similar to the existing condition.

Figure 2 – Catharine Street South and Forest Avenue Daylight Triangle

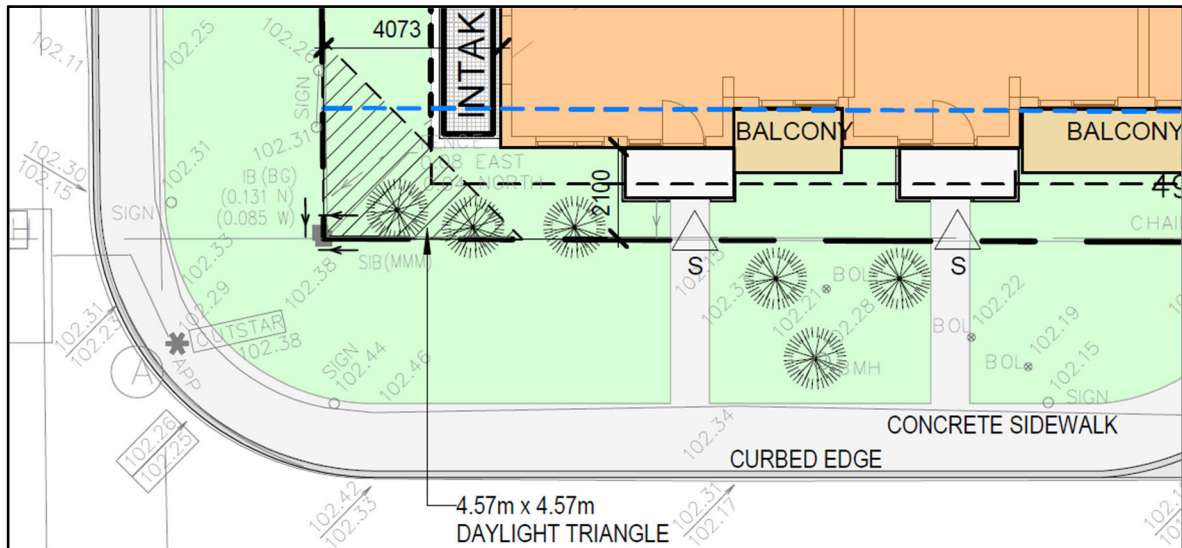


Figure 3 – Catharine Street South and Young Street



Source: Google Street View

9. The existing easterly driveway access to Young Street has been indicated as being 'retrofitted' in order to allow for 'one inbound and one outbound lane (approximately 3± m each) according to the submitted TA. Additional details to be illustrated on the future Site Plan and/or Transportation Assessment Letter are required indicating the proposed movements via the existing eastern driveway access that is proposed to lead to the underground parking from Young Street as illustrated in Figure 1. This existing driveway access is narrow and is currently non-conforming to city standards in order to facilitate two-way movements (the existing eastern Young Street driveway access is approximately 4.8± metres in width).

Response: The proposed site plan has been revised to provide a general 6m width underground vehicle ramp width that connects to Young Street. The narrowest part will be the overhead door with 5.5m width (at the bottom of the ramp). However, given that the inbound and outbound vehicle will be slowing down or stop at this location, this width should be sufficient for two-way movement. The City has indicated that this is acceptable in Comment #11 below. The revised site plan is included in **Appendix B**.

Therefore, this comment has been addressed.

10. 3 metre x 3 metre visibility triangles have been illustrated on the site plan at newly proposed access points along Forest Avenue. Transportation Planning notes that partial minor encroachments into the visibility triangle are proposed at the western driveway access, however it is expected that there will no significant affect on driver visibility when entering/exiting the driveway access point. Transportation Planning notes that no object or mature vegetation can exceed a height of 0.6 metres above the corresponding perpendicular centreline elevation of the adjacent street.

Response: Noted and no further action is required on this comment.

11. The proposed 'Covered Ramp to Underground Concrete Terrace' is too narrow to allow for two-way movements based on approximate dimensions measured from the most recent site plan submitted as illustrated in Figure 2. Transportation Planning indicates that interim discussions between the Applicant and City staff have noted a discrepancy in the overhead door width. A corrected interim submission site plan illustrated in Figure 3 indicates the overhead door width shall be 5.5 metres. Typically, a minimum of 6.0 metres is required to facilitate two-way movements internal to private property. Notwithstanding and without setting precedent, Transportation Planning will accept the proposed overhead door width of 5.5 metres, as the overall number of parking stalls is low and there is a long approach throat length extending from Young Street that will allow for vehicles to queue while waiting for opposing vehicles to maneuver in/out of the underground parking area.

Response: Noted and no further action is required for this comment.

12. A minimum of 3.0 metres x 3.0 metres visibility triangles should be provided for each driveway access. They must be illustrated, dimensioned and identified on the site plan. Visibility triangles are between the driveway limits and the ultimate property line (right-of-way limit) and no object or mature vegetation can exceed a height of 0.6 metres above the corresponding perpendicular centreline elevation of the adjacent street.

Notwithstanding and without setting precedent, Transportation Planning will accept reduced visibility triangle dimensions however the Applicant shall consider visibility restrictions when designing the at-grade features of the subject lands in the areas where a typical 3.0 metre x 3.0 metre visibility triangle would be located, as illustrated in Figure 2.

Transportation Planning recommends mirrors or warning systems to be implemented within this area to assist in facilitating the two-way movement of vehicles entering/exiting the underground parking area.

Response: Noted. This requirement has been addressed in the revised site plan (Appendix B) and Figure 4 below.

Figure 4 – Daylight Triangle Requirements for Proposed Driveways

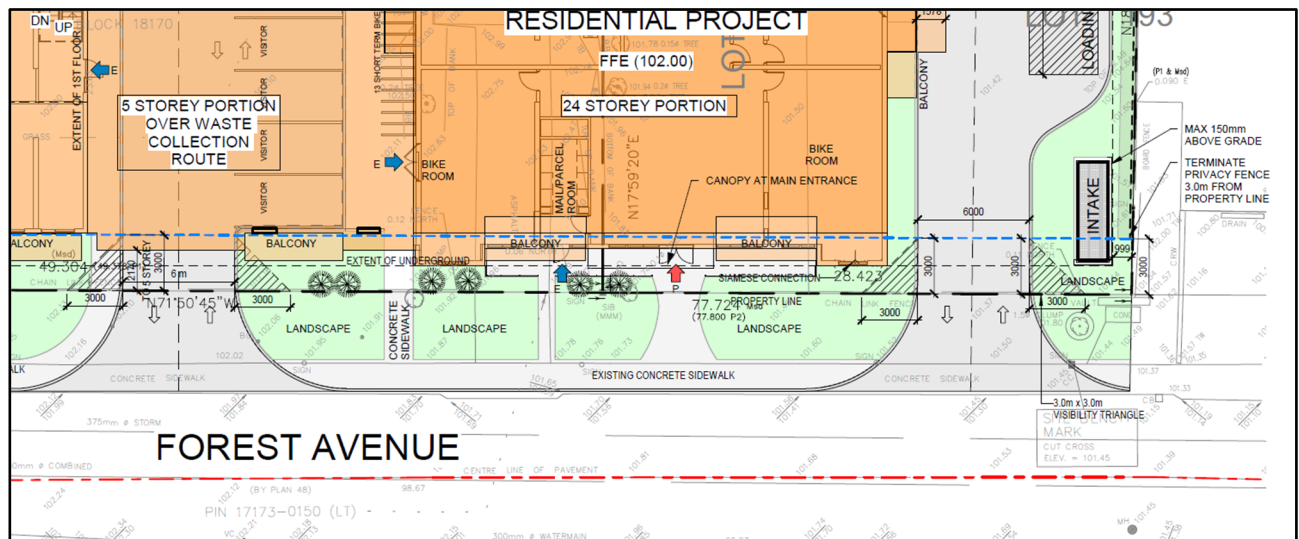
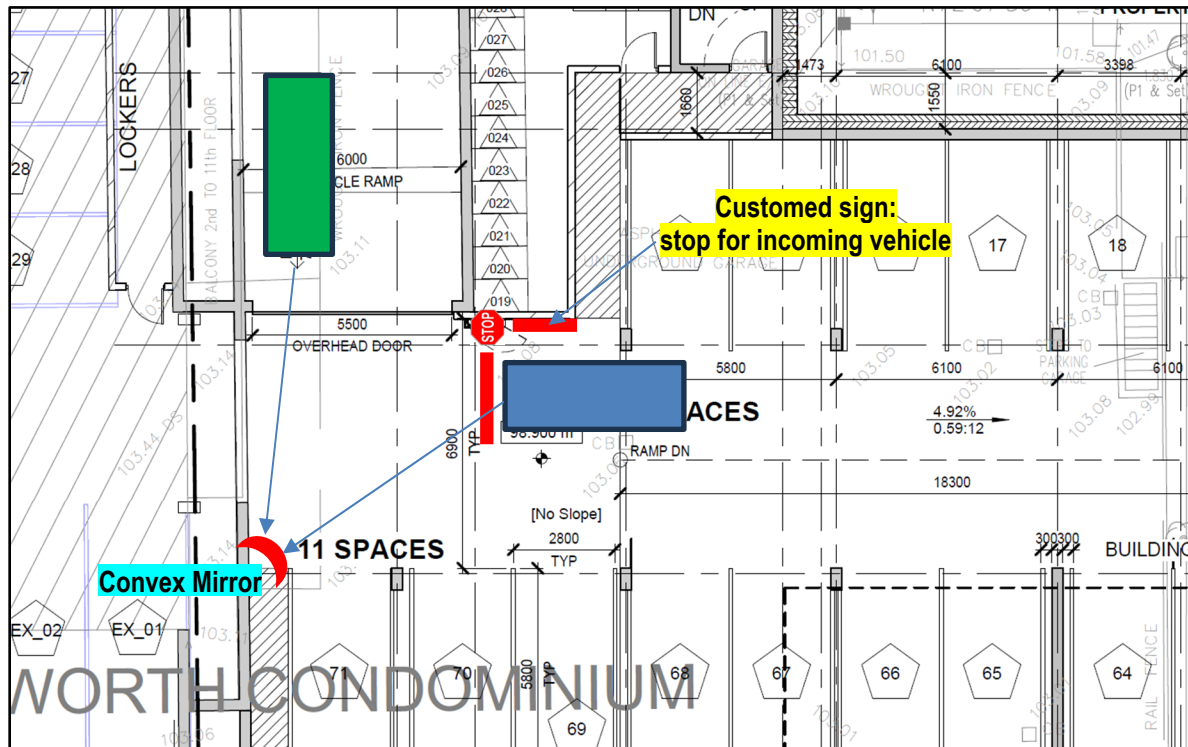


Figure 5 – Convex Mirrors and Warning Signage



2.0 CONCLUSIONS

Based on the additional assessment and findings provided in this Response Letter, all of the City comments have been addressed. It should be noted that the traffic calming costs will be provided as part of the site plan agreement. In addition, it should be noted that the daylight triangle requirement for Catharine Street South and Young Street are not possible at this time due to the existing building transformer and load bearing wall of the existing building that will be remained as part of the redevelopment of the site.

We trust the enclosed sufficiently addresses your needs. Should you have any questions, please do not hesitate to contact the undersigned.

Yours truly,

Nextrans Consulting Engineers

A Division of NextEng Consulting Group Inc.

Prepared by:

Sam Nguyen, Dipl.
Transportation Analyst

Reviewed and Approved by:

Richard Pernicky, MITE
Principal

Appendix A

City of Hamilton Comments



November 20, 2024

Attention: Alaina Baldassarra, Planner I, Development Planning

Prepared By: Matthew Radaelli, Project Manager, Transportation Planning

SUBJECT: 117 Forest Avenue and 175 Catharine Street South, Hamilton (Ward 2)
ZAC-23-019

- Transportation Assessment – **APPROVED (April 22, 2024)**
- Neighbourhood Traffic Calming Options – **FUNDS REQUIRED**
- Transportation Demand Management – **MEASURES REQUIRED**
- Right-of-Way Dedications – **NOT REQUIRED**
- Daylighting Triangle Dedications – **REQUIRED**
- Site Plan – **REVISIONS REQUIRED**

Documents Reviewed

- *Transportation Study Addendum 117 Forest Avenue and 175 Catharine Street South*, dated July 25, 2024, prepared by NexTrans Consulting Engineers.
- *117 Forest Ave & 175 Catherine St S, Hamilton Comment Response Chart*, undated.
- *SITE PLAN*, Drawing #A0.01, Revision #4, dated July 29, 2022.
- *UNDERGROUND LEVEL 1-3 PLAN*, Drawing #A1.01 – A1.03, Revision #4, dated July 29, 2022.

Transportation Assessment – Approved (April 22, 2024)

1. Transportation Planning notes a Transportation Assessment (TA) was previously submitted and approved as of April 22, 2024. It was noted that any revisions to the site plan to include additional residential units shall require an updated TA in order to reflect the proposed modifications to the site plan moving forward.
2. A revised Transportation letter was provided as part of the formal submission package. Transportation Planning notes that the letter appears to be ‘incomplete’ as references to figures (**Figures 5A & 5B**) and comments indicating certain revisions to the site plan (**Comment #9** indicating revisions and additional details provided at the driveway access to Young Street) have not been completed.



Neighbourhood Traffic Calming Options – Funds Required

3. Considering the increase in density being proposed within the subject lands, Transportation Planning shall require a provision of funds to the amount of \$10,000 for the purpose of installing future traffic calming measures within the surrounding area of the proposed development, subject to the satisfaction and approval of the Manager, Transportation Planning as part of the future conditions of Site Plan approval.

At a time when the Applicant is prepared to provide the required funds, Transportation Planning will correspond with the Transportation Operations & Maintenance department to provide an account number for the funds to be deposited accordingly.

Transportation Demand Management – Measures Required

4. Transportation Planning requires short-term and long-term bicycle parking to be provided as per the requirements of the City of Hamilton Comprehensive Zoning By-law 05-200, Section 5: Parking. All short-term and long-term bicycle parking shall be clearly indicated, illustrated and the total number of spaces shall be numbered on the site plan accordingly.

Additional Measures Recommended:

5. Transportation Planning recommends additional Transportation Demand Management (TDM) measures to be provided:
 - Provision of preloaded Presto/Transit passes with the purchase of an individual residential unit;
 - Reducing the number of passenger vehicle parking stalls provided to the minimum required through the City of Hamilton Comprehensive Zoning By-Law 05-200, Section 5: Parking;
 - Unbundled the cost of an individual parking stall from the cost of a residential unit; and
 - Provision of Electric Vehicle (EV) charging stalls within the underground parking area.

Right-of-Way Dedications – Not Required

6. The existing right-of-way at the subject property along Catharine Street South and Forest Avenue is approximately ± 20.0 metres. Transportation Planning does not require right-of-way dedications to be provided as the existing right-of-way width meets the requirements of the Council Approved Urban Official Plan: Chapter C - City Wide Systems and Designations, 4.5 Road Network Functional Classification, 4.5.2. Local Roads (Catharine Street South and Forest Avenue)



are to be 20.117 metres.

Daylighting Triangle Dedications – Required

7. Catharine Street South & Forest Avenue are both Local Roads. The Applicant is to dedicate a 4.57 metres x 4.57 metres Daylighting Triangle to the right-of-way, as per the Council Approved Urban Official Plan: Chapter C - City Wide Systems and Designations 4.5 Road Network Functional Classification; Daylighting Triangles 4.5.7.
8. Catharine Street South & Young Street are both Local Roads. The Applicant is to dedicate a 4.57 metres x 4.57 metres Daylighting Triangle to the right-of-way, as per the Council Approved Urban Official Plan: Chapter C - City Wide Systems and Designations 4.5 Road Network Functional Classification; Daylighting Triangles 4.5.7.

Site Plan – Revisions Required

Driveway Access

The following revisions are required:

9. The existing easterly driveway access to Young Street has been indicated as being 'retrofitted' in order to allow for 'one inbound and one outbound lane (approximately 3± m each) according to the submitted TA.

Additional details to be illustrated on the future Site Plan and/or Transportation Assessment Letter are required indicating the proposed movements via the existing eastern driveway access that is proposed to lead to the underground parking from Young Street as illustrated in **Figure 1**.

This existing driveway access is narrow and is currently non-conforming to city standards in order to facilitate two-way movements (the existing eastern Young Street driveway access is approximately 4.8± metres in width).



Figure 1: Existing Driveway Access to Young St to be Revised

Additional Information:

10. 3 metre x 3 metre visibility triangles have been illustrated on the site plan at newly proposed access points along Forest Avenue. Transportation Planning notes that partial minor encroachments into the visibility triangle are proposed at the western driveway access, however it is expected that there will no significant affect on driver visibility when entering/exiting the driveway access point. Transportation Planning notes that no object or mature vegetation can exceed a height of 0.6 metres above the corresponding perpendicular centreline elevation of the adjacent street.

Internal Circulation

The following revisions are required:

11. The proposed 'Covered Ramp to Underground Concrete Terrace' is too narrow to allow for two-way movements based on approximate dimensions measured from the most recent site plan submitted as illustrated in **Figure 2**.

Transportation Planning indicates that interim discussions between the Applicant and City staff have noted a discrepancy in the overhead door width. A corrected interim submission site plan illustrated in **Figure 3** indicates the overhead door width shall be 5.5 metres. Typically, a minimum of 6.0 metres is required to facilitate two-way movements internal to private property. Notwithstanding and without setting precedent, Transportation Planning will accept the proposed overhead door width of 5.5 metres, as the overall number of parking stalls is low and there is a long approach throat length extending from Young Street that will allow for vehicles to queue while waiting for opposing vehicles to maneuver in/out of the underground parking area.



Transportation Planning recommends mirrors or warning systems to be implemented within this area to assist in facilitating the two-way movement of vehicles entering/exiting the underground parking area.

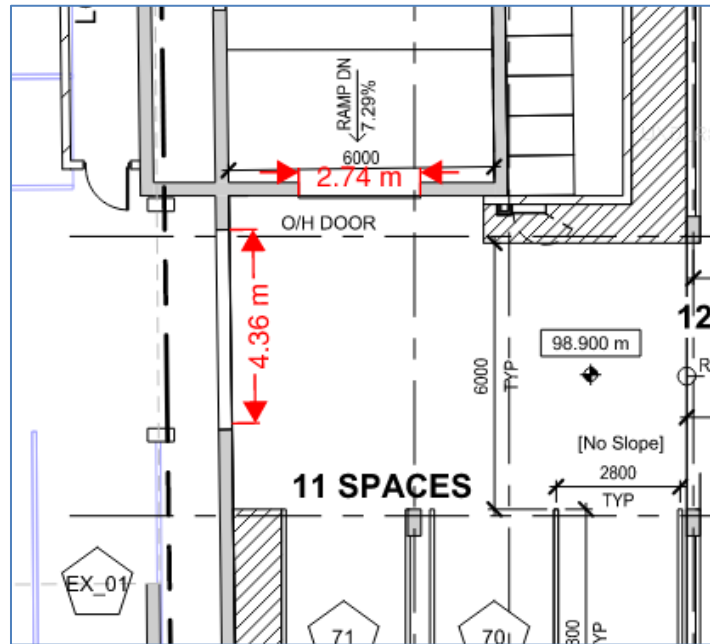


Figure 2: Subject Submission Overhead Door Width

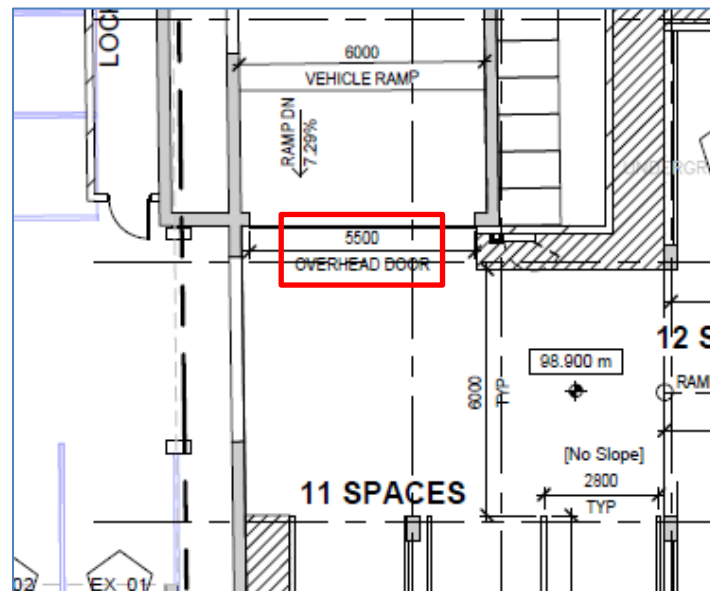


Figure 3: Revised Site Plan Overhead Door Width (November 18, 2024 Site Plan)

Should you have any questions, please email tplanning@hamilton.ca, referencing:



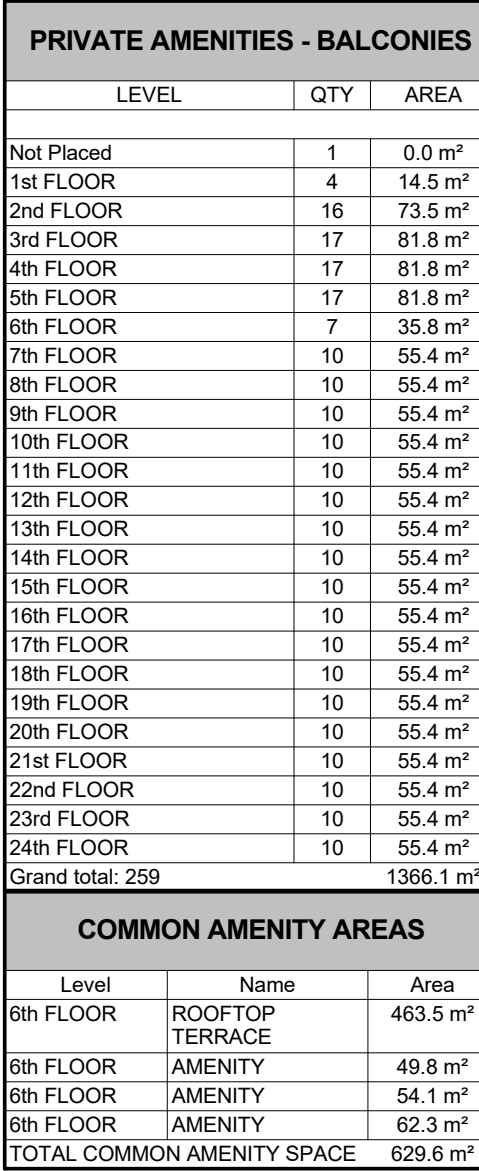
Hamilton

117 Forest Ave & 175 Catharine St S – ZAC-23-019 (Ward 2) Transportation Planning
Response

cc: Development Engineering Approvals

Appendix B

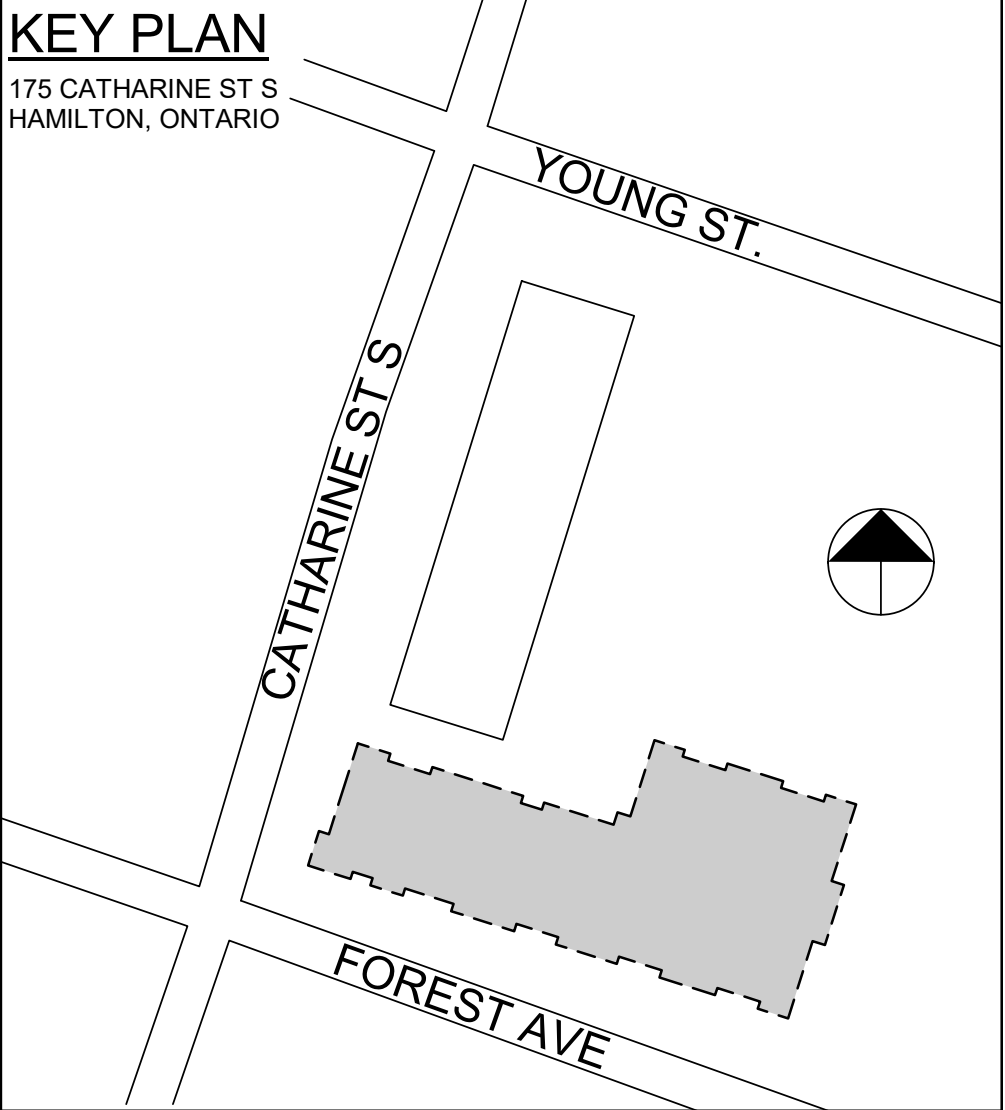
Proposed Site Plan



| PARKING OVERALL | | |
|--|--------------------|-------|
| Level | Parking Space Type | Count |
| Total Visitor Parking Spaces | | |
| 1st FLOOR | Barrier-Free | 1 |
| | Small | 2 |
| | Standard | 10 |
| | | 13 |
| Total Tenant Parking Spaces (Existing) | | |
| UNDERGROUND LEVEL 1 | Standard | 29 |
| Total Tenant Parking Spaces | | |
| UNDERGROUND LEVEL 1 | Barrier-Free | 4 |
| UNDERGROUND LEVEL 1 | Standard | 54 |
| UNDERGROUND LEVEL 2 | Small | 3 |
| UNDERGROUND LEVEL 2 | Standard | 62 |
| Grand total | | 123 |
| | | 165 |

| BIKE PARKING | | | |
|--------------------|---------------|--------------------|---------------------|
| | UNIT COUNT | SPACES PER UNIT | REQUIRED PARKING |
| LONG TERM PARKING | 256 | 0.5 | 128 SPACES |
| SHORT TERM PARKING | 256 | 0.05 | 13 SPACES |

| LEVEL | DIMENSIONS (mm) | | QTY |
|-----------------------|-----------------|--------|-----|
| | WIDTH | LENGTH | |
| BIKE / STORAGE LOCKER | | | |
| 1st FLOOR | 900 | 1,500 | 65 |
| UNDERGROUND LEVEL 1 | 900 | 1,500 | 52 |
| UNDERGROUND LEVEL 2 | 900 | 1,500 | 18 |
| | | | 135 |











| SITE LANDSCAPE AREA | | | |
|---------------------|----------------------|-----------------------|-------------|
| | Area | | |
| | (m ²) | (ft ²) | (acre) |
| Existing | 73.7 m ² | 794 ft ² | 0.018 acres |
| New | 645.7 m ² | 6,950 ft ² | 0.160 acres |
| Landscape Area | 719.4 m ² | 7,744 ft ² | 0.178 acres |

| Level | Area (m²) | Area (ft²) |
|--------------|-------------------------------|--------------------------------|
| 1st FLOOR | 756.03 m ² | 8,137.8 ft ² |
| 2nd FLOOR | 756.03 m ² | 8,137.8 ft ² |
| 3rd FLOOR | 756.03 m ² | 8,137.8 ft ² |
| 4th FLOOR | 756.03 m ² | 8,137.8 ft ² |
| 5th FLOOR | 756.03 m ² | 8,137.8 ft ² |
| 6th FLOOR | 756.03 m ² | 8,137.8 ft ² |
| 7th FLOOR | 756.03 m ² | 8,137.8 ft ² |
| 8th FLOOR | 756.03 m ² | 8,137.8 ft ² |
| 9th FLOOR | 756.03 m ² | 8,137.8 ft ² |
| 10th FLOOR | 756.03 m ² | 8,137.8 ft ² |
| 11th FLOOR | 566.40 m ² | 6,096.7 ft ² |
| | 8,126.67 m² | 87,474.8 ft² |

| GFA - PROPOSED BUILDING | | |
|-------------------------|-------------|--------------|
| Level | Area (m²) | Area (ft²) |
| 1st Floor | 817.2 m² | 8,796.1 SF |
| 2nd Floor | 1,224.5 m² | 13,260.5 SF |
| 3rd Floor | 1,227.3 m² | 13,261.3 SF |
| 4th Floor | 1,227.3 m² | 13,261.3 SF |
| 5th Floor | 1,227.3 m² | 13,261.3 SF |
| 6th Floor | 710.1 m² | 7,643.0 SF |
| 7th Floor | 686.6 m² | 7,390.5 SF |
| 8th Floor | 686.6 m² | 7,390.5 SF |
| 9th Floor | 686.6 m² | 7,390.5 SF |
| 10th Floor | 686.6 m² | 7,390.5 SF |
| 11th Floor | 686.6 m² | 7,390.5 SF |
| 12th Floor | 686.6 m² | 7,390.5 SF |
| 13th Floor | 686.6 m² | 7,390.5 SF |
| 14th Floor | 686.6 m² | 7,390.5 SF |
| 15th Floor | 686.6 m² | 7,390.5 SF |
| 16th Floor | 686.6 m² | 7,390.5 SF |
| 17th Floor | 686.6 m² | 7,390.5 SF |
| 18th Floor | 686.6 m² | 7,390.5 SF |
| 19th Floor | 686.6 m² | 7,390.5 SF |
| 20th Floor | 686.6 m² | 7,390.5 SF |
| 21st Floor | 686.6 m² | 7,390.5 SF |
| 22nd Floor | 686.6 m² | 7,390.5 SF |
| 23rd Floor | 686.6 m² | 7,390.5 SF |
| 24th Floor | 686.6 m² | 7,390.5 SF |
| Total of GFA | 18,792.3 m² | 202,278.7 SF |

| GROSS SELLABLE AREA | | |
|---------------------|------------|------------------------|
| Floor | # of Units | Areas |
| 1st FLOOR | 4 | 229.9 m ² |
| 2nd FLOOR | 0 | 191.3 m ² |
| 2nd FLOOR | 4 | 84.5 m ² |
| 3rd Floor | 17 | 1049.7 m ² |
| 4th Floor | 17 | 1049.7 m ² |
| 5th Floor | 17 | 1049.7 m ² |
| 6th FLOOR | 7 | 408.7 m ² |
| 7th FLOOR | 10 | 589.0 m ² |
| 8th FLOOR | 10 | 589.0 m ² |
| 9th FLOOR | 10 | 589.0 m ² |
| 10th FLOOR | 10 | 589.0 m ² |
| 11th FLOOR | 10 | 589.0 m ² |
| 12th FLOOR | 10 | 589.0 m ² |
| 13th FLOOR | 10 | 589.0 m ² |
| 14th FLOOR | 10 | 589.0 m ² |
| 15th FLOOR | 10 | 589.0 m ² |
| 16th FLOOR | 10 | 589.0 m ² |
| 17th FLOOR | 10 | 589.0 m ² |
| 18th FLOOR | 10 | 589.0 m ² |
| 19th FLOOR | 10 | 589.0 m ² |
| 20th FLOOR | 10 | 589.0 m ² |
| 21st FLOOR | 10 | 589.0 m ² |
| 22nd FLOOR | 10 | 589.0 m ² |
| 23rd FLOOR | 10 | 589.0 m ² |
| 24th FLOOR | 10 | 589.0 m ² |
| Total of GSA | 266 | 15426.6 m ² |

LEGEND

| | |
|---|---|
|  | PROPERTY LINE |
| P  | "PRINCIPAL ENTRANCE" AS PER ONTARIO BUILDING CODE AND FIRE ACCESS/FIRE CODE |
| E  | ENTRY/ EXITS DOOR |
| W  | WASTE SERVICE DOOR |
| S  | ENTRY TO SINGLE SUITE FROM GROUND LEVEL |
|  | EXTENT OF UNDERGROUND |
|  | FIRE ROUTE |
|  | WASTE COLLECTION ROUTE |

| SITE STATISTICS | |
|---------------------------------------|---|
| GENERAL INFORMATION (EXISTING) | |
| BUILDING AREA | 2756m ² |
| BUILDING HEIGHT | ±38m |
| # OF STOREYS | 11 |
| # OF RESIDENTIAL UNITS | 11 |
| # OF UNDERGROUND LEVELS | 1 |
| FLOOR AREA RATIO | |
| 175 Catherine St S | 8,126.67 m ² |
| Proposed Building | 18,792.31 m ² |
| Total | 26,919.98 m ² |
| Site Area | 4,530.5m ² |
| Floor Area Ratio (FAR) | 5.94 m ² |
| SITE STATISTICS | |
| GENERAL INFORMATION (PROPOSED) | |
| BUILDING AREA | 1227.3 m ² |
| BUILDING HEIGHT | 74.4m |
| # OF STOREYS | 24 |
| # OF RESIDENTIAL UNITS | 256 |
| # OF UNDERGROUND LEVELS | 2 |
| SITE AREA | 4,530.5m ² (shared/combined) |

| UNIT BREAKDOWN PER FLOOR | | | | |
|--------------------------|-------------|-------------|-------------|-------------|
| Level | 1 BED UNITS | 2 BED UNITS | 3 BED UNITS | TOTAL UNITS |
| 1st FLOOR | 0 | 3 | 1 | 4 |
| 2nd FLOOR | 9 | 5 | 0 | 14 |
| 3rd FLOOR | 10 | 7 | 0 | 17 |
| 4th FLOOR | 10 | 7 | 0 | 17 |
| 5th FLOOR | 10 | 7 | 0 | 17 |
| 6th FLOOR | 4 | 3 | 0 | 7 |
| 7th FLOOR | 6 | 4 | 0 | 10 |
| 8th FLOOR | 6 | 4 | 0 | 10 |
| 9th FLOOR | 6 | 4 | 0 | 10 |
| 10th FLOOR | 6 | 4 | 0 | 10 |
| 11th FLOOR | 6 | 4 | 0 | 10 |
| 12th FLOOR | 6 | 4 | 0 | 10 |
| 13th FLOOR | 6 | 4 | 0 | 10 |
| 14th FLOOR | 6 | 4 | 0 | 10 |
| 15th FLOOR | 6 | 4 | 0 | 10 |
| 16th FLOOR | 6 | 4 | 0 | 10 |
| 17th FLOOR | 6 | 4 | 0 | 10 |
| 18th FLOOR | 6 | 4 | 0 | 10 |
| 19th FLOOR | 6 | 4 | 0 | 10 |
| 20th FLOOR | 6 | 4 | 0 | 10 |
| 21st FLOOR | 6 | 4 | 0 | 10 |
| 22nd FLOOR | 6 | 4 | 0 | 10 |
| 23rd FLOOR | 6 | 4 | 0 | 10 |
| 24th FLOOR | 6 | 4 | 0 | 10 |
| TOTAL UNITS | 151 | 104 | 1 | 256 |
| Type of Unit | Total | Percentage | | |
| 1 BED | 147 | 57.42% | | |
| 2 BED | 108 | 42.18% | | |
| 3 BED | 1 | 0.39% | | |
| TOTAL | 256 | 100.00% | | |


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| | |
|---|----------------|
|  | DETAIL NUMBER |
| | DRAWING NUMBER |

| DRAWING SETS ISSUED | No. | DATE (DD,MM,YY) | BY |
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| ISSUED FOR SPA | 1. | 23.04.19 | KNY/MH |
| ISSUED FOR SPA | 2. | 10.05.21 | KNY/MH |
| SPA COMMENTS | 3. | 10.08.21 | KNY/MH |
| ISSUED FOR BP APPROVAL | 4. | 29.07.22 | KNY/MH |

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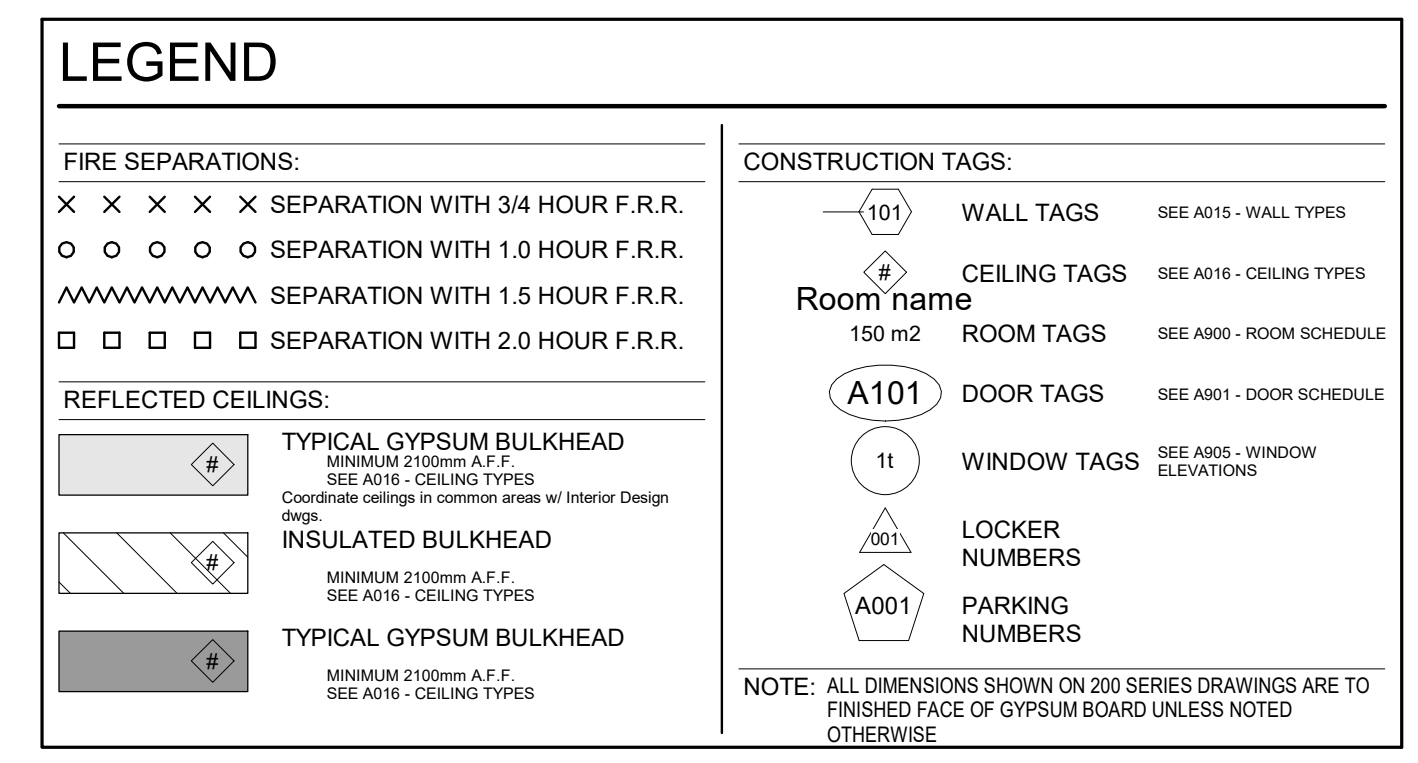
**BROCKTON
APARTMENTS**

175 Catharine St S
Hamilton, Ontario

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| SITE PLAN | |
| DRAWING SCALE: | PROJECT NUMBER: |

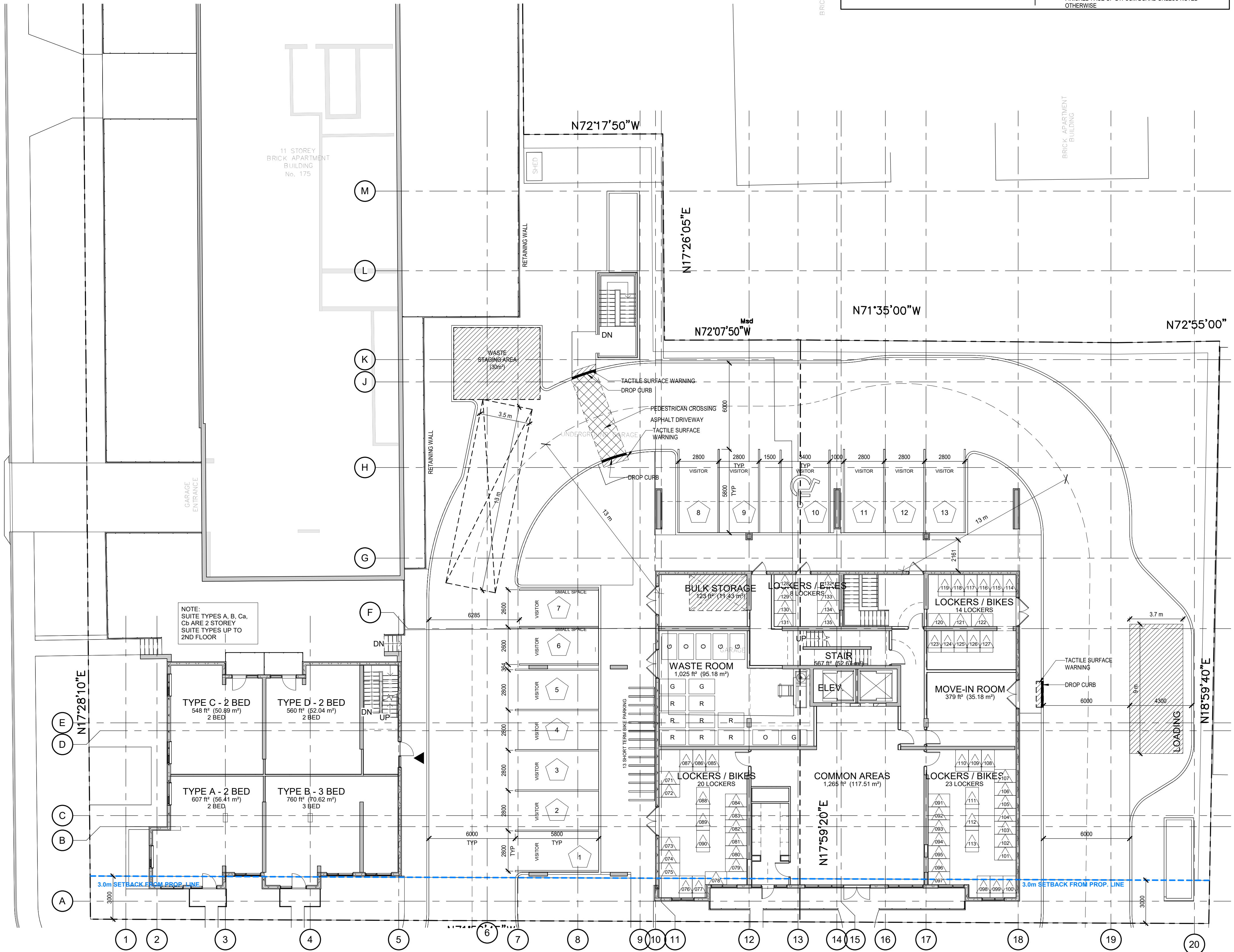
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| As indicated | 21039 |
| DRAWN BY | CHECKED BY |
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| PLOT DATE: 2025-01-13 | |



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| DRAWING VERSION: | | |
| PLOT DATE: 2025-01-13 | | |

TIME: 2025-01-13 1:25:03 AM FILEPATH: C:\Rock\Loc\21039 Brockton Apartments_3D\3D\Arch\A1.02.dwg



LEGEND

FIRE SEPARATIONS:

- X X X X SEPARATION WITH 3/4 HOUR F.R.R.
O O O O SEPARATION WITH 1.0 HOUR F.R.R.
~~~~~ SEPARATION WITH 1.5 HOUR F.R.R.  
□ □ □ □ SEPARATION WITH 2.0 HOUR F.R.R.

### REFLECTED CEILINGS:

- TYPICAL GYPSUM BULKHEAD  
MINIMUM 150mm A.F.F.  
SEE A016 - CEILING TYPES  
Coordinate ceilings in common areas w/ Interior Design Dept.
- INSULATED BULKHEAD  
MINIMUM 2100mm A.F.F.  
SEE A016 - CEILING TYPES
- TYPICAL GYPSUM BULKHEAD  
MINIMUM 2100mm A.F.F.  
SEE A016 - CEILING TYPES

### CONSTRUCTION TAGS:

- 101 WALL TAGS SEE A015 - WALL TYPES
- Room name  
150 m2 CEILING TAGS SEE A016 - CEILING TYPES
- A101 ROOM TAGS SEE A000 - ROOM SCHEDULE
- 11 DOOR TAGS SEE A001 - DOOR SCHEDULE
- 11 WINDOW TAGS SEE A005 - WINDOW ELEVATIONS
- LOCKER NUMBERS
- PARKING NUMBERS

NOTE: ALL DIMENSIONS SHOWN ON 200 SERIES DRAWINGS ARE TO FINISHED FACE OF GYPSUM BOARD UNLESS NOTED OTHERWISE

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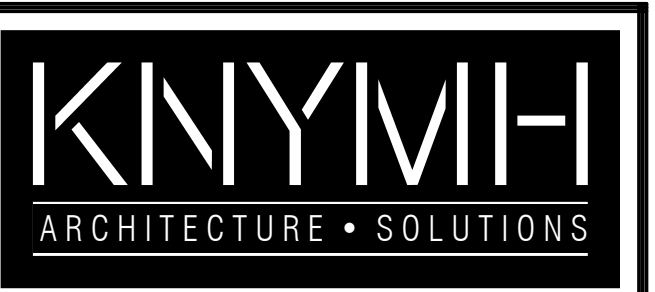
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| No. | DRAWING SHEET NUMBER |

| DRAWING SETS ISSUED    | No. | DATE (DD.MM.YY) | BY    |
|------------------------|-----|-----------------|-------|
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| ISSUED FOR SPA         | 2.  | 10.05.21        | KNYMH |
| SPA COMMENTS           | 3.  | 10.08.21        | KNYMH |
| ISSUED FOR BP APPROVAL | 4.  | 29.07.22        | KNYMH |

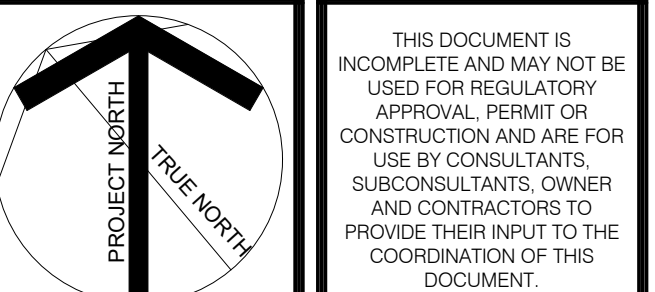
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**BROCKTON APARTMENTS**  
175 Catharine St S  
Hamilton, Ontario

DRAWING SHEET TITLE:

**1ST FLOOR PLAN**

DRAWING SCALE:

As indicated

PROJECT NUMBER:

**21039**

DRAWN BY:

CHECKED BY:

DRAWING SHEET NUMBER:

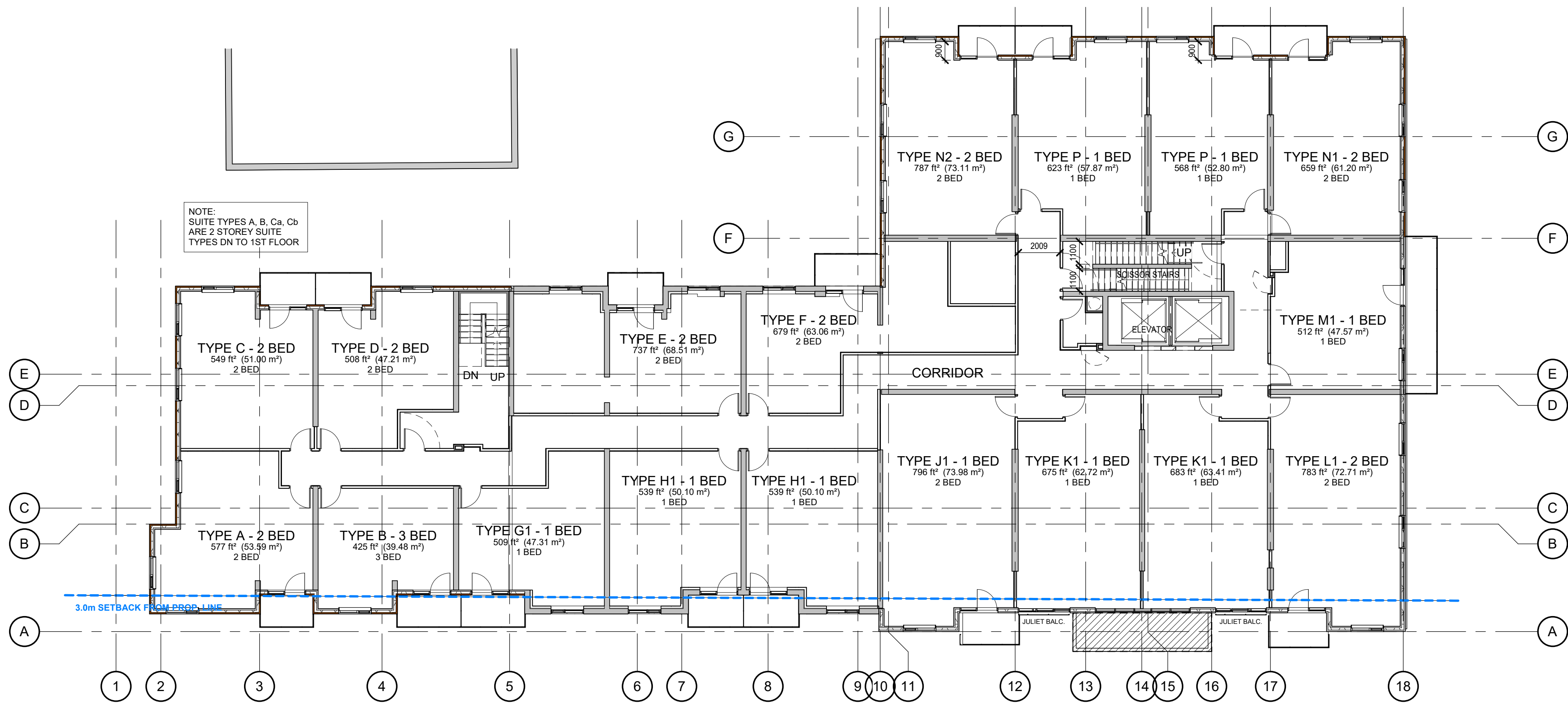
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**A1.02**

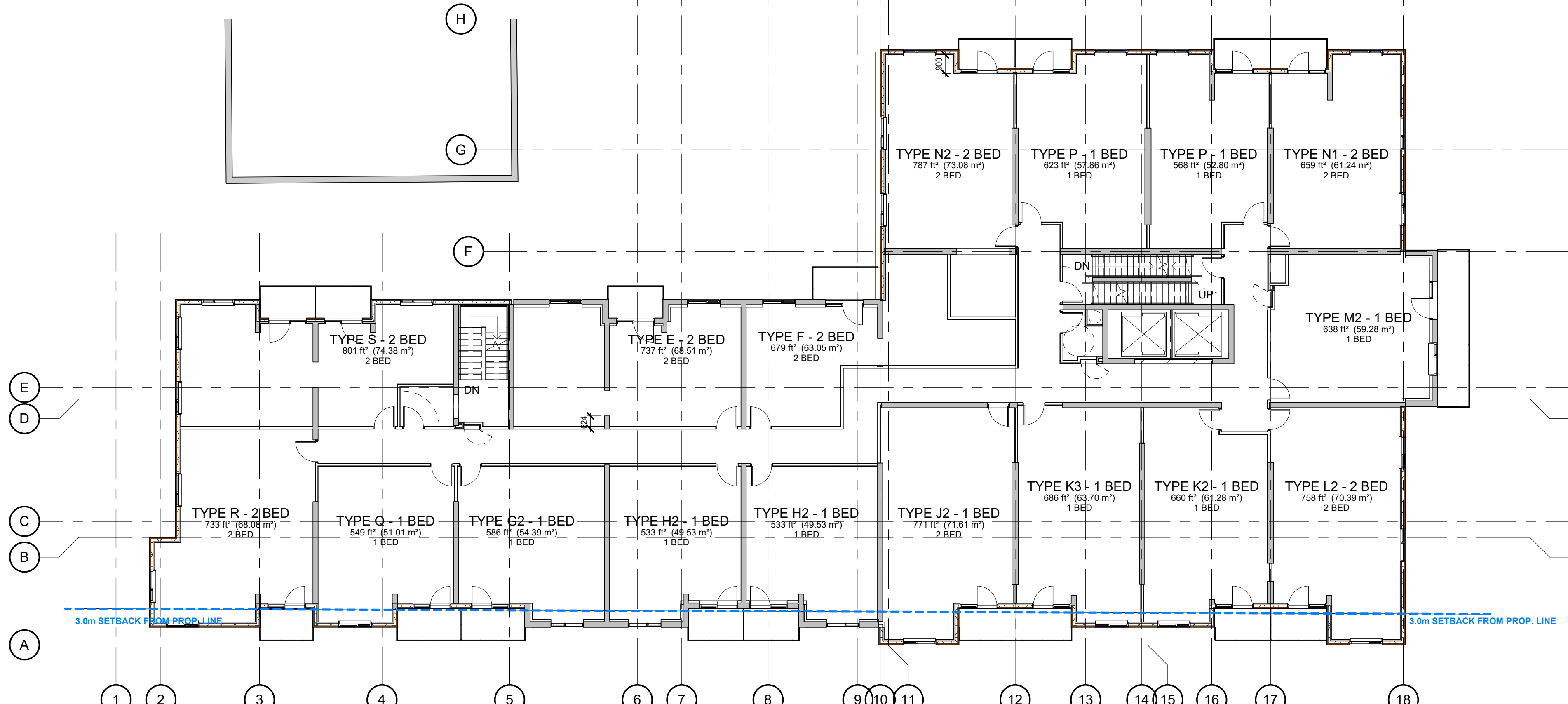
PLOT DATE:

2025-01-13

**1 1st FLOOR PLAN**  
A1.02 1 : 150



1 2nd FLOOR PLAN  
A1.03 1:150



2 3rd-5th FLOOR PLAN  
A1.03 1:150

## LEGEND

### FIRE SEPARATIONS:

- X X X X SEPARATION WITH 3/4 HOUR F.R.R.
- O O O O SEPARATION WITH 1.0 HOUR F.R.R.
- ~~~~~ SEPARATION WITH 1.5 HOUR F.R.R.
- □ □ □ SEPARATION WITH 2.0 HOUR F.R.R.

### REFLECTED CEILINGS:

- TYPICAL GYPSUM BULKHEAD  
MINIMUM 2100mm A.F.F.  
SEE A016 - CEILING TYPES  
Coordinate ceilings in common areas w/ Interior Design Dept.
- INSULATED BULKHEAD  
MINIMUM 2100mm A.F.F.  
SEE A016 - CEILING TYPES
- TYPICAL GYPSUM BULKHEAD  
MINIMUM 2100mm A.F.F.  
SEE A016 - CEILING TYPES

### CONSTRUCTION TAGS:

- 101 WALL TAGS SEE A015 - WALL TYPES
- # CEILING TAGS SEE A016 - CEILING TYPES
- Room name ROOM TAGS SEE A000 - ROOM SCHEDULE
- A101 DOOR TAGS SEE A001 - DOOR SCHEDULE
- 1t WINDOW TAGS SEE A005 - WINDOW ELEVATIONS
- 501 LOCKER NUMBERS
- A001 PARKING NUMBERS

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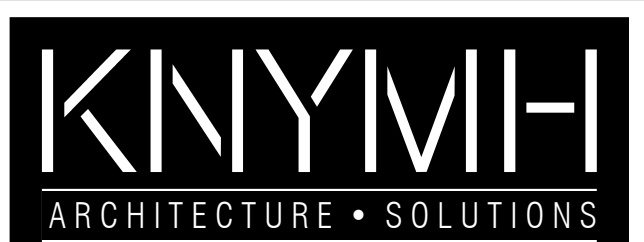
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| ISSUED FOR SPA         | 1.  | 23.04.19        | KNYMH |
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| SPA COMMENTS           | 3.  | 10.08.21        | KNYMH |
| ISSUED FOR BP APPROVAL | 4.  | 29.07.22        | KNYMH |

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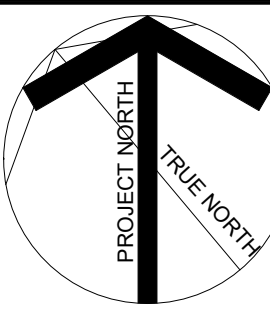
REVISIONS TO DRAWING No. DATE (DD.MM.YY)

| No. | DATE (DD.MM.YY) | BY |
|-----|-----------------|----|
| No. | DATE (DD.MM.YY) | BY |

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175 Catharine St S  
Hamilton, Ontario

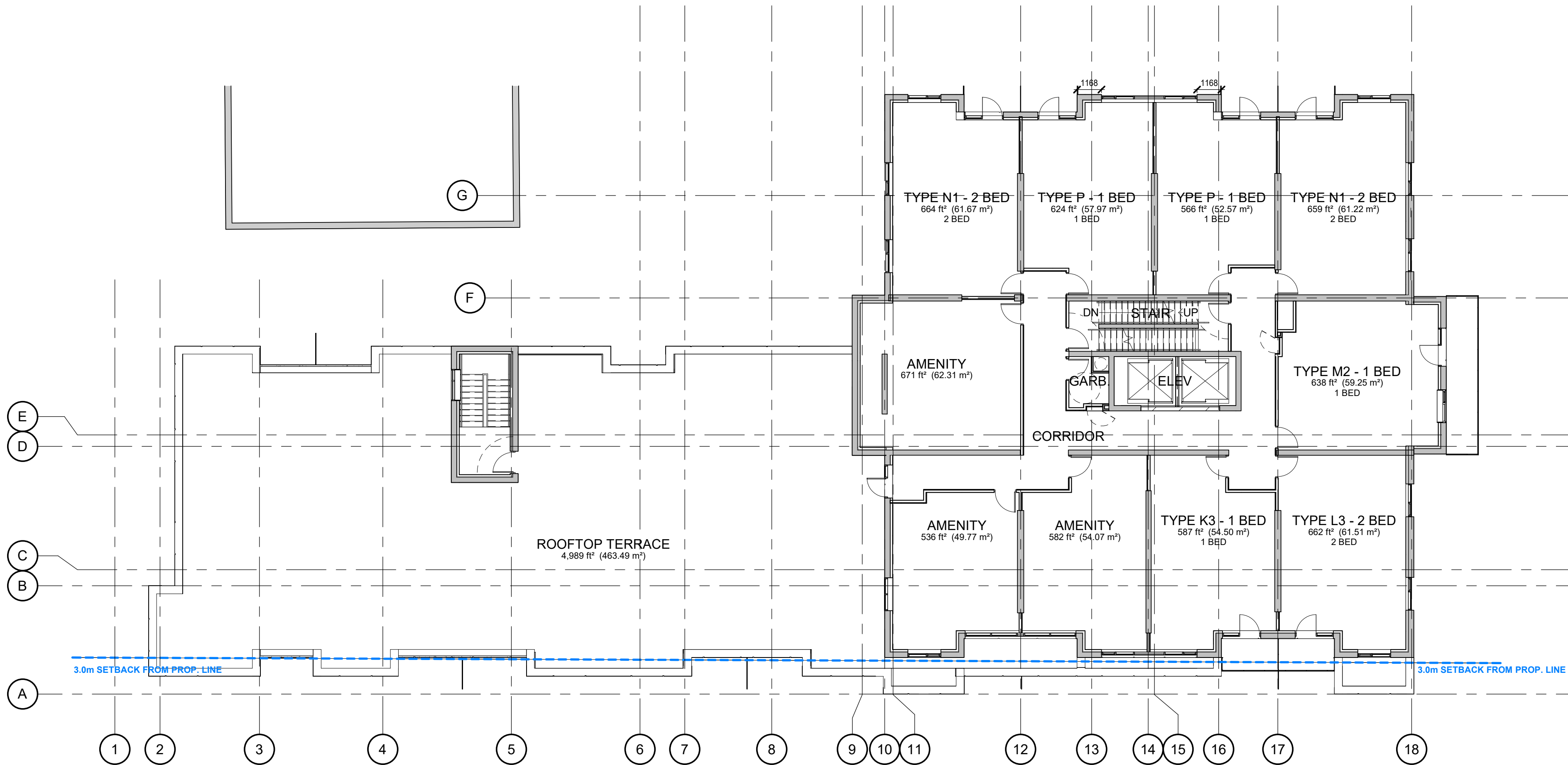
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2ND TO 5TH FLOOR PLANS

DRAWING SCALE:  
As indicated

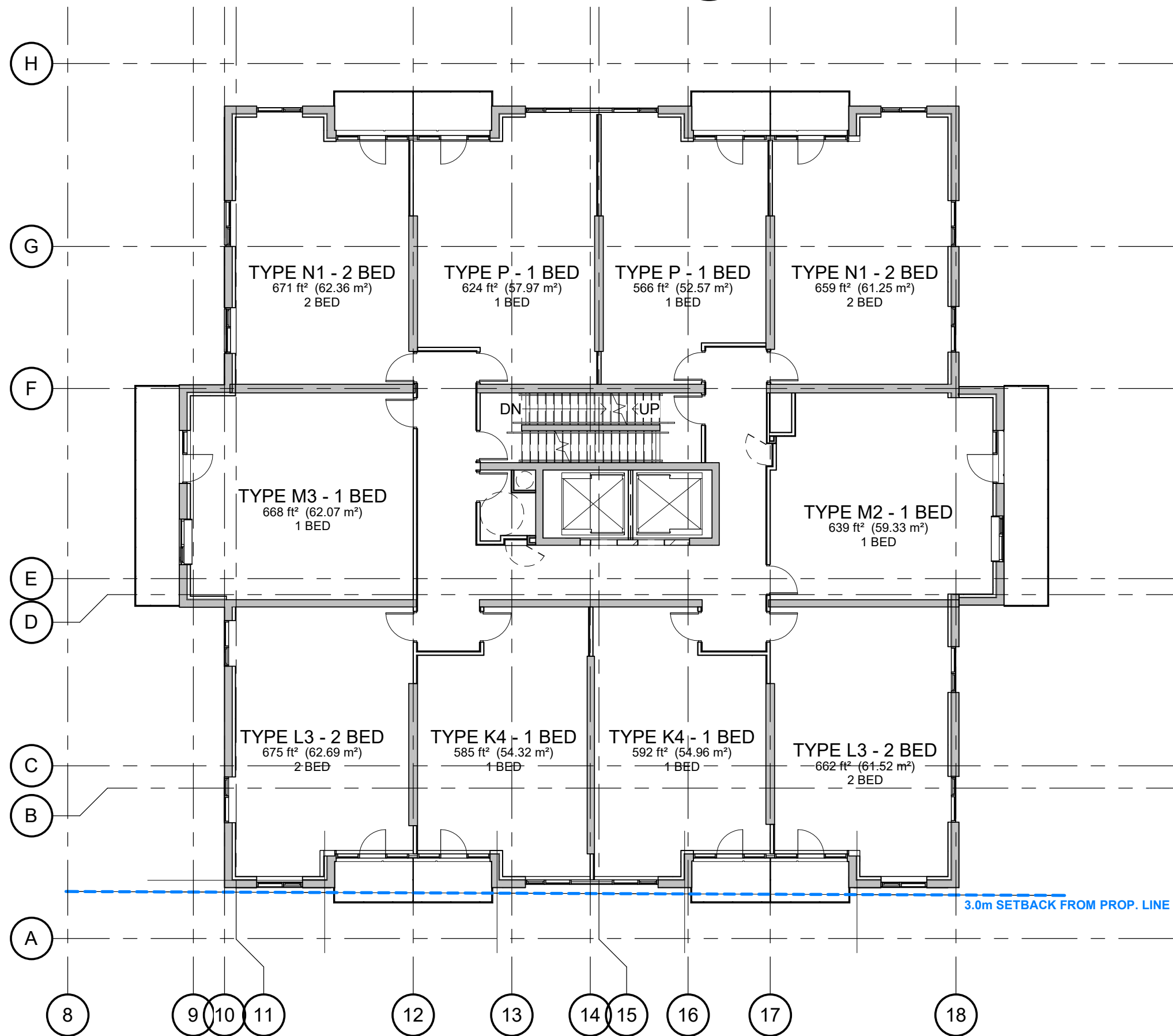
DRAWN BY: CHECKED BY: PROJECT NUMBER:  
21039

DRAWING VERSION:  
PLOT DATE:  
2025-01-13

A1.03



1 6th FLOOR PLAN  
A1.04 1 : 150



2 7th-24th FLOOR PLAN  
A1.04 1 : 150

## LEGEND

### FIRE SEPARATIONS:

- × × × × SEPARATION WITH 3/4 HOUR F.R.R.  
○ ○ ○ ○ SEPARATION WITH 1.0 HOUR F.R.R.  
~~~~~ SEPARATION WITH 1.5 HOUR F.R.R.  
□ □ □ □ SEPARATION WITH 2.0 HOUR F.R.R.

REFLECTED CEILINGS:

- TYPICAL GYPSUM BULKHEAD
MINIMUM 210mm A.F.F.
SEE A016 - CEILING TYPES
Coordinate ceilings in common areas w/ Interior Design Dept.
INSULATED BULKHEAD
MINIMUM 210mm A.F.F.
SEE A016 - CEILING TYPES
TYPICAL GYPSUM BULKHEAD
MINIMUM 210mm A.F.F.
SEE A016 - CEILING TYPES

CONSTRUCTION TAGS:

- 101 WALL TAGS SEE A016 - WALL TYPES
Room name
150 m2 CEILING TAGS SEE A016 - CEILING TYPES
A101 ROOM TAGS SEE A000 - ROOM SCHEDULE
1t DOOR TAGS SEE A001 - DOOR SCHEDULE
501 WINDOW TAGS SEE A005 - WINDOW ELEVATIONS
LOCKER NUMBERS
A001 PARKING NUMBERS

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KEY TO DETAIL LOCATION

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| ISSUED FOR BP APPROVAL | 4. | 29.07.22 | KNYMH |

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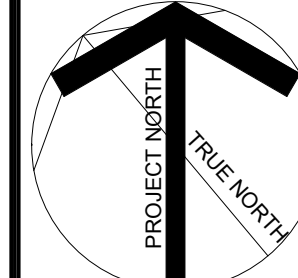
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175 Catharine St S
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DRAWING SHEET TITLE:

6TH TO 24TH FLOOR PLANS

DRAWING SCALE:

As indicated

PROJECT NUMBER:

21039

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CHECKED BY:

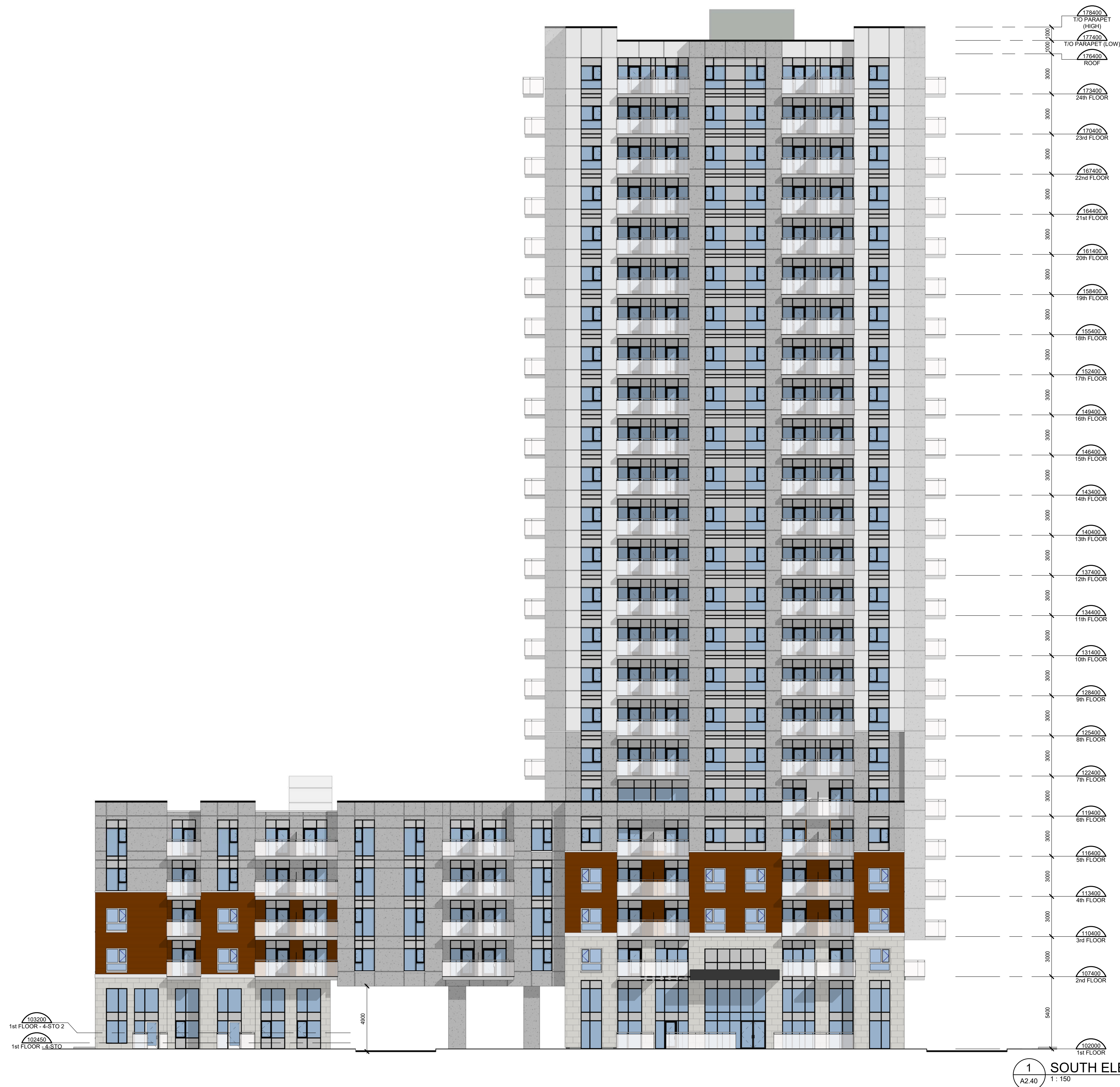
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DRAWING VERSION:

A1.04

PLOT DATE:

2025-01-13



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KEY TO DETAIL LOCATION

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| No. | DETAIL NUMBER |
| No. | DRAWING SHEET NUMBER |

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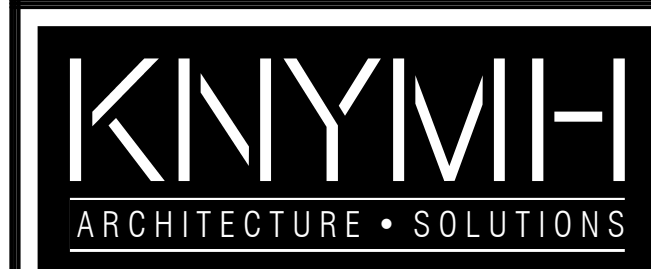
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(MM/DD,MM,YY)
BY

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BROCKTON
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DRAWING SHEET TITLE:

SOUTH ELEVATION
(FOREST AVE)

DRAWING SCALE:

1:15

PROJECT NUMBER

21039

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| DRAWN BY: | |
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DRAWING SHEET NUMBER

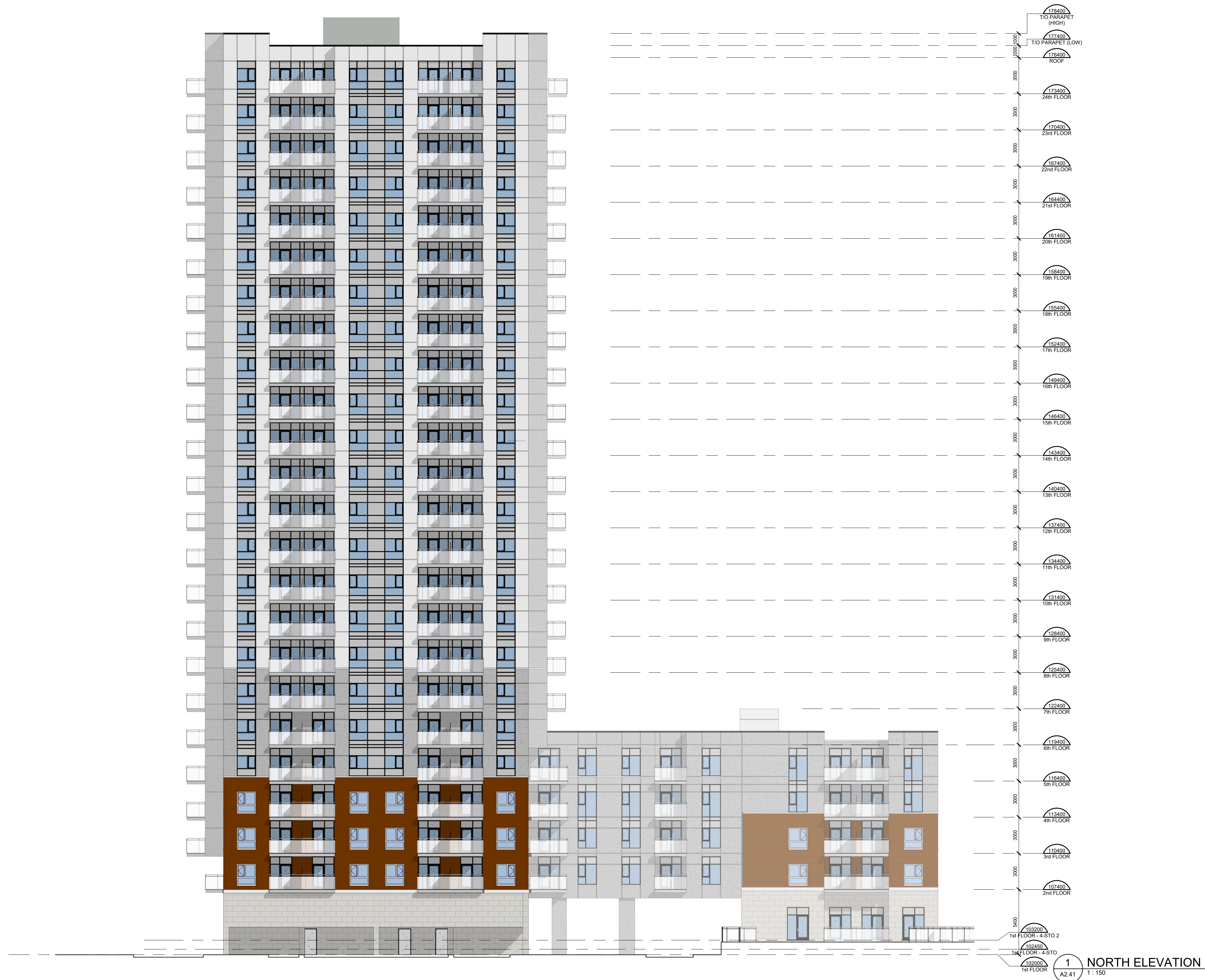
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DATE: _____

A2 40

A2.40

TIMESTAMP: 2025-01-13 11:25:38 AM FILEPATH: C:\Revit Local\21039 Brockton-Apartments_3.0mStudy_michaelm\N935K.nt




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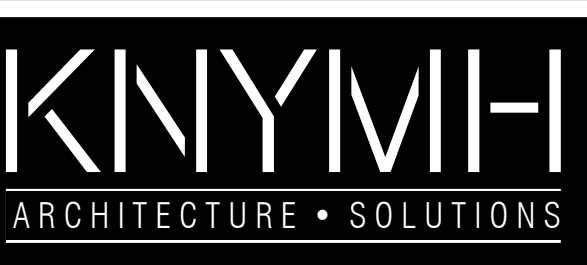

 No. DETAIL NUMBER
 No. DRAWING SHEET NUMBER

| DRAWING SETS ISSUED | No. | DATE (DD,MM,YY) | BY |
|------------------------|-----|--------------------|------|
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| COMMENTS | 3. | 10.08.21 | KNYM |
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BROCKTON APARTMENTS

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Hamilton, Ontario

WORKING SHEET TITLE:

NORTH ELEVATIONS

SCORING SCALE:

1 : 150

PROJECT NUMBER

21039

DRAWN BY:

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DRAWING SHEET NUMBER

...WING VERSION

A2.41

DATE: 2025-01-13



WING VERSION: A2.42
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